

ITEM	12
MANAGER'S REPORT NO.	43
COUNCIL MEETING	88/06/20

RE: METROTOWN AREA 11 PLAN  
 POTENTIAL EXPANSION OF THE AREA  
 DESIGNATED FOR RM1 DEVELOPMENT PROGRESS REPORT

MUNICIPAL MANAGER'S RECOMMENDATION:

1. THAT the recommendation of the Director Planning & Building Inspection be adopted.

\* \* \* \* \*

TO: MUNICIPAL MANAGER 1988 JUNE 14

FROM: DIRECTOR PLANNING &  
 BUILDING INSPECTION

SUBJECT: METROTOWN AREA 11 PLAN  
 POTENTIAL EXPANSION OF THE AREA  
 DESIGNATED FOR RM1 DEVELOPMENT  
 PROGRESS REPORT

RECOMMENDATIONS:

1. THAT copies of this report be sent to United Properties, Columbia Housing and the residents of the Christopher Court Area who have participated in recent meetings with staff and the potential developers of the site.

R E P O R T

1.0

BACKGROUND

On 1988 April 18, Council received a report from the Planning and Building Inspection Department that recommended that no change be made to the existing adopted Metrotown Area 11 boundaries at the present time.

Council referred the report and instructed staff to work with United Properties, Columbia Housing and the residents of the Christopher Court Area in an effort to find a "creative" solution to the redevelopment of the area.

2.0

MEETINGS BETWEEN RESIDENTS, THE DEVELOPERS AND STAFF

Meetings were held at Inman School on 1988 April 28 and 1988 May 12 with 15 residents of Christopher Court, a representative from United Properties and their architect, a representative from Columbia Housing and a member of the Planning staff. The purpose of the meetings was to allow for an exchange of ideas and to seek a solution to the development of the three large lots which front on Boundary Road west of Christopher Court.

At the meetings, the developers outlined their situation with regard to them having received preliminary approval for a cooperative project from C.M.H.C. while the residents outlined their concerns regarding their expectations about preservation of the low density residential character of their street along with other issues such as the completion of the cul-de-sac and provision for the separation of traffic between their street and any future townhouse development.

135

After considerable discussion involving the number of residential units, the location of vehicular access to a potential townhouse area and the provision of a landscape buffer area between the new and existing development, the majority of concerns expressed by the Christopher Court residents were addressed. The residents were able to reach a consensus with the developer that if the Christopher Court cul-de-sac could be extended westward to provide a completed cul-de-sac bulb with four more single family lots enclosing the end of the street, the residents could possibly accept a townhouse development on the remaining approximately two-thirds of the site to the west with appropriate separate access to Thurston. This represented a considerable change from the opening views expressed by many of the residents.

### 3.0

#### OUTSTANDING ISSUES

##### 3.1 ACCESS

The developer, to date, has not yet been able to negotiate the purchase of the Thurston Street lots in order to expand the site southward to Thurston or to acquire one privately-owned lot to provide a driveway between Thurston and the three large lots to the north. The developer is proposing that he be sold a Municipal lot on Thurston which has 100 feet of frontage on which he would develop a driveway, along with approximately eight townhouse units. While staff can support a 50 foot wide access driveway from Thurston in light of the difficulties associated with the accessibility of the site, the development of eight townhouses on what is basically one single lot on Thurston Avenue, detached from the balance of the townhouse development to the north, is not supported by staff. The creation of a row of townhouses on a single lot within a block face which is presently exclusively single family housing is not considered appropriate for rezoning purposes. Redevelopment of the area should take place within an orderly framework and within a logical development pattern if the impacts of new development are to be kept within reasonable limits.

##### 3.2 DENSITY

The developer has, up to this point, not submitted detailed design plans for the site. The density permitted however in RM1 areas provides for a 0.45 Floor Area Ratio where underground parking is not provided or a maximum of 0.60 Floor Area Ratio where the parking is entirely underground. The developer has prepared a preliminary design with the parking areas partially exposed where the driving aisles are open to the air. This approach does not provide usable open space on top of the parking deck, hence does not constitute underground parking and as such is not eligible for the bonus. The developer has been advised of this situation and is reviewing his proposal.

##### 3.3 SETBACKS FROM BOUNDARY ROAD

As Boundary Road is to be upgraded in the near future as a primary arterial road and is a designated truck route, a 50 foot building setback between the new property line and any new multiple housing is considered a minimum desirable environmental standard adjacent Boundary Road. Depending upon the orientation and building design, minor encroachments of up to 10 feet may be considered.

ITEM	12
MANAGER'S REPORT NO.	43
COUNCIL MEETING	88/06/20

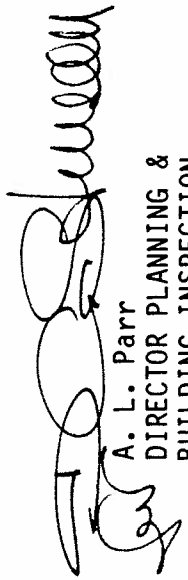
4.0 CONCLUSIONS

While the primary concerns of the residents of Christopher Court have been addressed to a significant degree through an intensive discussion process, two issues (those of access to the potential townhouse area from Thurston and the preparation of a suitable plan with the appropriate siting and density) remain to be resolved.

In the interests of providing a solution which is acceptable to both the residents of Christopher Court and the residents along Thurston Street and to provide a standard of development compatible with their areas, it is necessary to prepare a development plan using the RM1 zone guidelines. The consideration of development proposals which do not meet basic zoning requirements and land assembly standards is not appropriate particularly when clear alternatives that will produce a compatible addition to the neighbourhood are available.

While a creative solution to a part of the problem has been achieved with a high level of cooperation from the residents immediately east of the site, there is an incumbent responsibility on the Municipality to ensure that the basic quality, zoning and land assembly issues are resolved in a manner which is consistent with other RM1 districts in the Municipality and which have been achieved for cooperative housing in nearby locations on Bond Street and other parts of Burnaby.

Staff will continue meeting with the developers in an effort to achieve a solution to the issues outlined in this report.



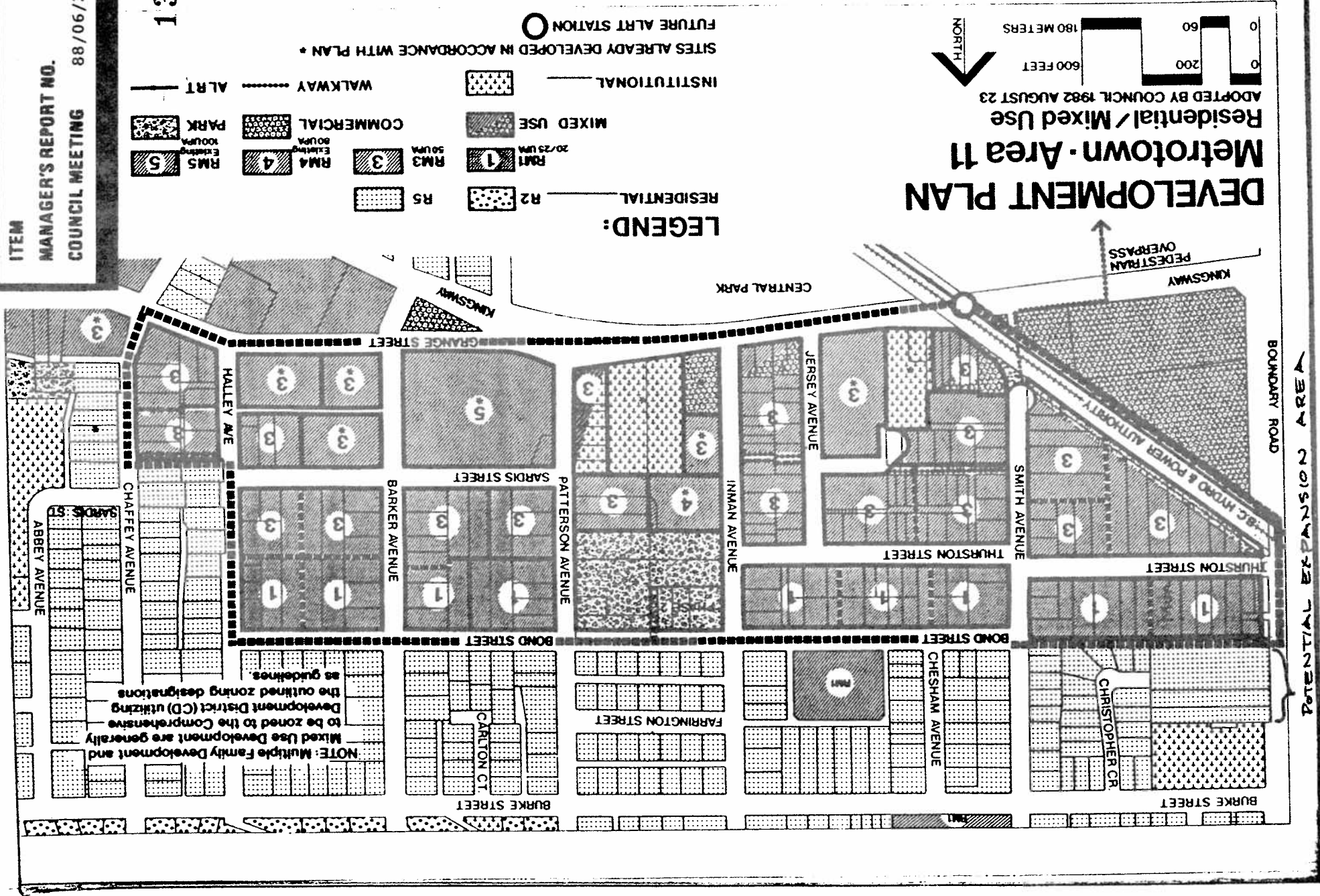
A. L. Parr  
DIRECTOR PLANNING &  
BUILDING INSPECTION

BR:ap

cc: Director Engineering

ITEM 12  
 MANAGER'S REPORT NO. 43  
 COUNCIL MEETING 88/06/20

137



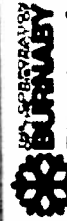
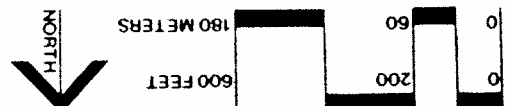
NOTE: Multiple Family Development and Mixed Use Development are generally to be zoned to the Comprehensive Development District (CD) utilizing the outlined zoning designations as guidelines.

**LEGEND:**

- R1 20/25 URM
- R2
- R3 RM3 50 URM
- R4 RM4 Existing 80 URM
- R5
- MIXED USE
- COMMERCIAL
- PARK
- RMS Existing 100 URM
- 5
- INSTITUTIONAL
- WALKWAY
- ALRT
- FUTURE ALRT STATION
- SITES ALREADY DEVELOPED IN ACCORDANCE WITH PLAN

**DEVELOPMENT PLAN**  
**Metrotown - Area 11**

Residential / Mixed Use  
 ADOPTED BY COUNCIL 1982 AUGUST 23



Planning &  
 Building Inspection  
 Department

Date: 00 JUNE

Scale:

Drawn By:

METROTOWN AREA # PLAN 2

SKETCH 1